BUILDING HEIGHT Maximum Building Height	12m				
BUILDING ENVELOPE					
	Setbacks (min)	Setbacks (min)			
	Where ≤8.5m in height	Where > 8.5m-12m in height			
RONTAGE SETBACKS (Primary F	rontage)				
ont Setback - To Wall	3.0m	6.0m			
ont Setback - To OMP	2.0m	5.0m			
ont Setback - To Covered Car Parking pace	5.4m	N/A			
RONTAGE SETBACKS (Secondar	y Frontage - Street)				
econdary Frontage Setback - To Wall	2.0m	3.0m			
Secondary Frontage Setback - To OMP	1.0m	2.0m			
econdary Frontage Setback - To covered Car Parking Space	5.4m	N/A			
RONTAGE SETBACKS (Secondar	y Frontage - Laneway)				
Secondary Frontage Setback (Laneway) - Fo Wall and OMP	0.5m	0.5m			
SIDE & REAR SETBACKS (Non Bui	ilt to Boundary)				
Side & Rear Setbacks (Non-BTB)	As Per QDC MP1.1 or MP1.2 (r	refer to Side Setbacks Table)			
BUILT TO BOUNDARY WALLS (0m	- 0.2m Side Setback)				
Vhere Lot Frontage is ≤7.5m	Mandatory both sides; Max. length 80% of boundary length & Max. BTB wall height of 7.5m				
Where Lot Frontage is >7.5m - 12.5m	Mandatory one side, as shown on the Approved Plan of Development; Max. length 60% of boundary length & Max. BTB wall height of 7.5m				
Vhere Lot Frontage is >12.5 - 18m	Optional one side, as shown on the Approved Plan of Development; Max. length of 15m or 60% of the length of the boundary (whichever is lesser) & Max. BTB wall height of 7.5m				
Vhere Lot Frontage is >18m	Not Permitted				
AXIMUM SITE COVER					
	Site Cover (max) Where ≤8.5m in height	Site Cover (max) Where > 8.5m-12m in height			
ot Area 300m² or less	75%	50%			
ot Area 301-401m²	70%	50%			
ot Area 401m²-500m²	60%	60%			
ot Area 501m² +	60% 50%				
GARAGE OPENINGS (Maximum W	idth)				
ot Frontage Less than 12.5m	Single Storey: Max. 50% of the frontage width (being the frontage vehicle access is from) and recessed min. 1.0m behind the main building line or min. 1.0m behind a front portico and no more than 2.0m in front of the main building line); Two Storey: Max. 6.0m wide and recessed 1.0m behind front wall or balcony of upper level				
Lot Frontage 12.5m - 18m	Max. 6.0n	n wide			
ot Frontage Greater than 18m	Not Spe	cified			
ADDITIONAL DESIGN REQUIREME	ENTS				
	Along built to boundary wall or otherwise	indicated on the Plan of Development			
referred Garage Location	1 per Dwellir	ng House			
	Maximum of 40% of the frontage access is being obtained from or 4.8m, whichever is the lesser				
referred Garage Location linimum Number of Car Spaces crossover Widths	Maximum of 40% of the frontage access is being	obtained from or 4.0m, whichever is the lesser			

urban planning, surveying

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SIDE SETBACKS TABLE					
Road Frontage (in metres)	Side Boundary Clearances (in metres)				
	≤4.5m building height	>4.5m to 7.5m bulding height	>7.5m to 10.5m building height	>10.5m to 12m building height	
15.001 or greater	1.500m	2.0m			
14.501-15.0	1.425m	1.9m			
14.001-14.5	1.350m	1.8m			
13.501-14.0	1.275m	1.7m			
13.001-13.5	1.200m	1.6m			
12.501-13.0	1.125m	1.5m	2.5m	3.0m	
12.001-12.5	1.050m	1.4m			
11.501-12.0	0.975m	1.3m			
11.001-11.5	0.900m	1.2m			
10.501-11.0	0.825m	1.1m			
10.5 or less	0.750m	1.0m			

Side setbacks shown above have been derived from QDC MP1.1 & MP1.2.

Design guidelines in table have been derived from the Dwelling House Code of the Moreton Bay Regional Council Planning Scheme V4 and are applicable to the design of dwelling houses within the subject site.

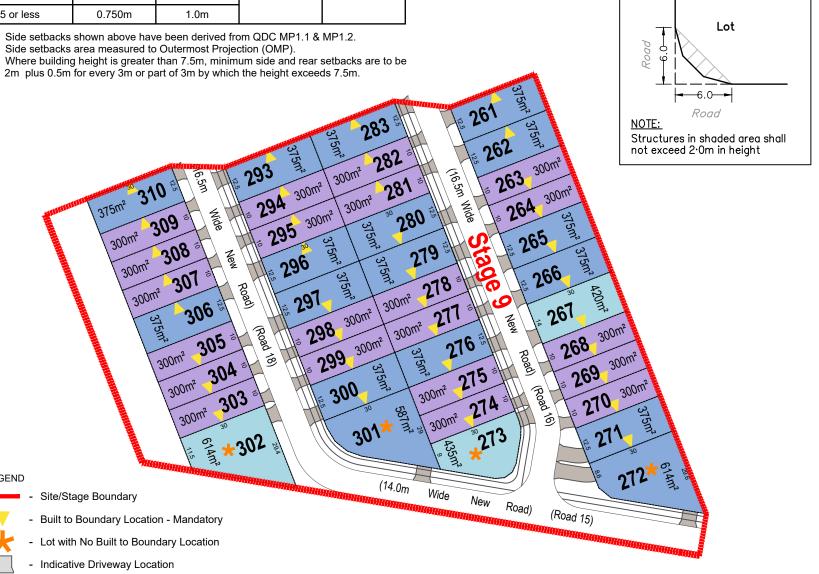
Additional requirements may apply to the design of dual-occupancy dwellings and dwelling houses with secondary dwellings.

Non-compliance with the siting and design provisions may require a concurrence agency response from Council

To be read in conjunction with the Approved Plan of Development for the subject

Where an inconsistency exists between a diagram on Sheets 2, 3, or 4 with the Dwelling House Design Guidelines and Side Setbacks Tables shown on Sheet 1, the the provisions outlined on Sheet 1 prevail to the extent of the inconsistency.

> Special siting requirements <u>for corner allotments</u>



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- MBRC Lot Type B

- MBRC Lot Type C

- MBRC Lot Type D

Road frontage setbacks (Primary and Secondary Frontage) for corner Lots to be determined using the relevant criteria stipulated within the Dwelling House Code (RAD3) and associated provisions within the Moreton Bay Regional Council Planning Scheme or other Planning Scheme as applicable at the time of construction. Specifically, note that the orientation of the 'principle pedestrian entrance' to the future dwelling can influence the determination of Primary Frontage and therefore associated setback provisions, under the Planning Scheme.

> **BUILDING ENVELOPE PLAN** |A3-6255/BLE| A **AMITY ESTATE** Project: BNE150725 STAGE 9 "AMITY" CALLAGHAN ROAD, NARANGBA B150725Sk12.dwg